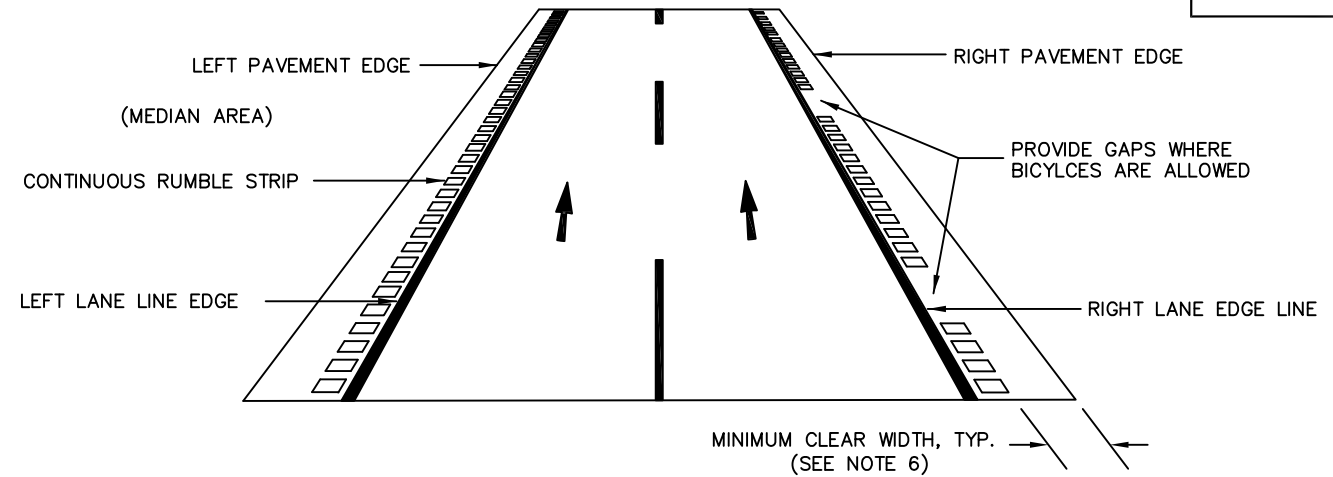


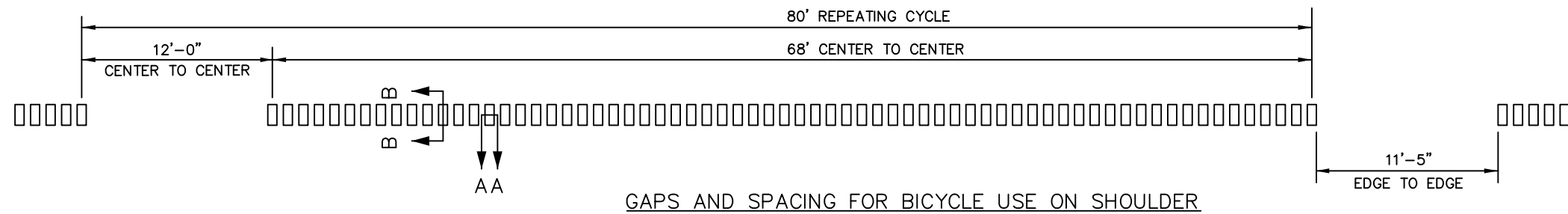
TYPICAL SHOULDER INSTALLATION – TWO-WAY  
PERSPECTIVE VIEW

APPLIES TO TWO-WAY OPERATION  
WHERE BICYCLES ARE ALLOWED

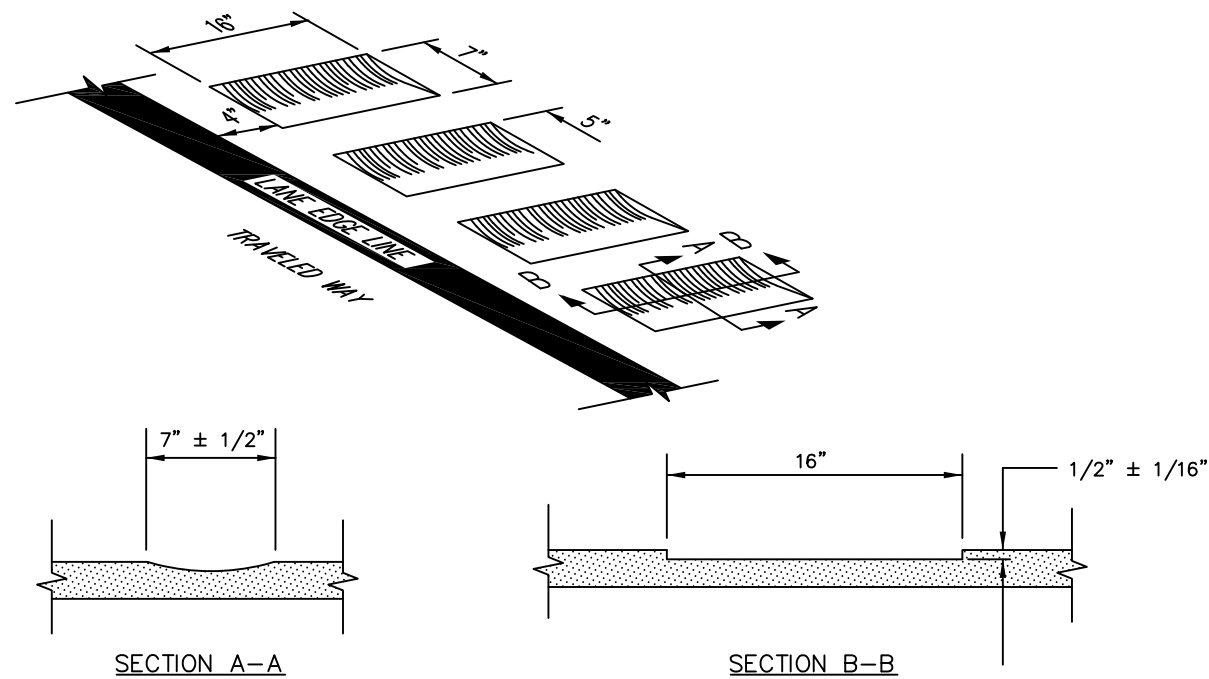


TYPICAL SHOULDER INSTALLATION – ONE-WAY DIVIDED  
PERSPECTIVE VIEW

APPLIES TO ONE-WAY DIVIDED HIGHWAYS  
WHERE BICYCLES ARE ALLOWED



GAPS AND SPACING FOR BICYCLE USE ON SHOULDER



TYPICAL SHOULDER INSTALLATION DETAIL

SHOULDER RUMBLE STRIP NOTES:

1. PERFORM ALL STAKING AS NECESSARY TO INSTALL RUMBLE STRIPS IN ACCORDANCE WITH THE PLANS, THESE DETAILS, AND THE FOLLOWING NOTES:
2. DO NOT INSTALL RUMBLE STRIPS IN THE FOLLOWING INSTANCES:
  - A. BRIDGE DECKS
  - B. BRIDGE APPROACH SLABS
  - C. PAVEMENT LESS THAN 2 INCHES THICK
  - D. PAVEMENT THAT HAS ALLIGATORING, FATIGUE, CRACKING, OR IN POOR CONDITION
  - E. PAVEMENT JOINTS
  - F. INTO LANE EDGE LINE STRIPING
3. USE CENTERLINE OR LANE LINE DIVIDING LINES, RATHER THAN LANE EDGE LINES, FOR RUMBLE STRIP ALIGNMENT CONTROL WHENEVER POSSIBLE.
4. WHERE BICYCLES ARE ALLOWED ON THE FACILITY, SHOULDER RUMBLE STRIP GAPS (68' RUMBLE STRIP, 12' GAP CENTER TO CENTER, 11'-5" GAP, EDGE TO EDGE) SHOULD BE CONTINUOUS.
5. ON DIVIDED HIGHWAYS, PROVIDE CONTINUOUS RUMBLE STRIP ON THE INSIDE (LEFT) SHOULDER.
6. MINIMUM REQUIRED CLEAR WIDTHS AFTER INSTALLATION ARE AS FOLLOWS:
  - A. AT LEAST 4' WHERE NO GUARDRAIL IS PRESENT (6.0' INITIAL SHOULDER WIDTH).
  - B. AT LEAST 5' (TO FACE OF GUARDRAIL) WHERE GUARDRAIL IS PRESENT (≥ 7.0' AT INITIAL SHOULDER WIDTH).
  - C. NO MINIMUM WHERE BICYCLES ARE PROHIBITED.

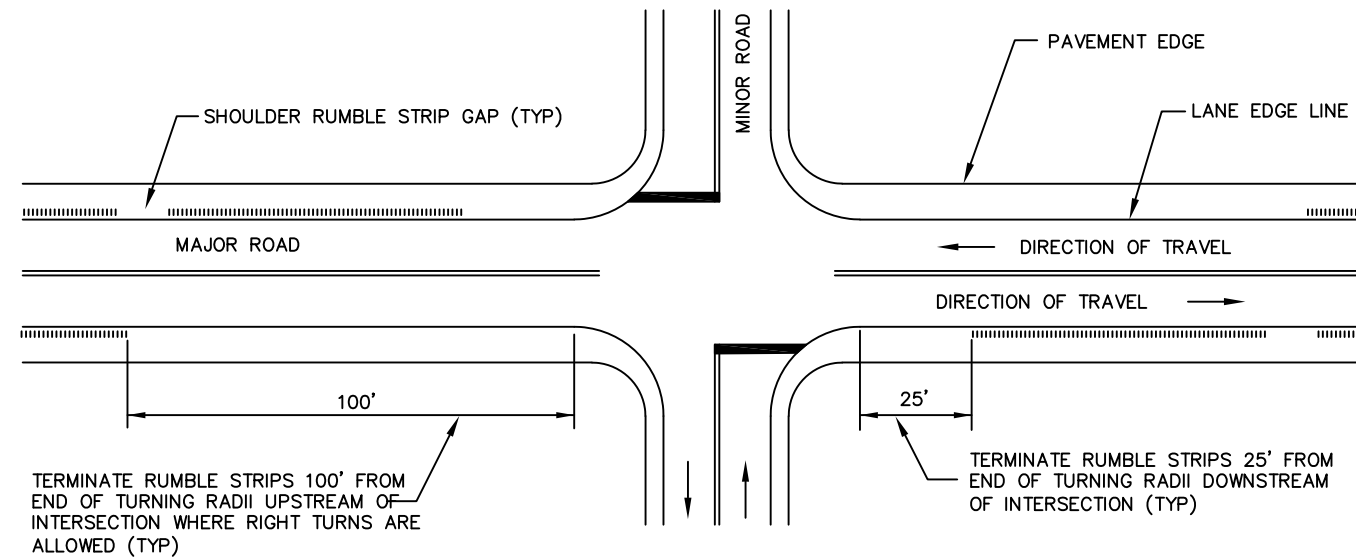
Note: Drawing not to scale

State of Alaska DOT&PF  
ALASKA STANDARD PLAN  
**MILLED RUMBLE STRIPS  
SHOULDER DETAILS**

Adopted as an Alaska Standard Plan by: *Carolyn Morehouse*  
Carolyn Morehouse, P.E.  
Chief Engineer

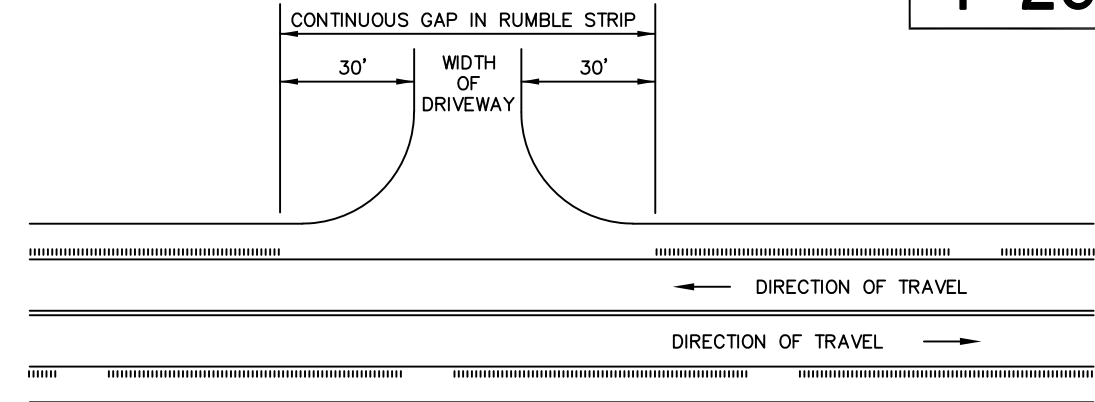
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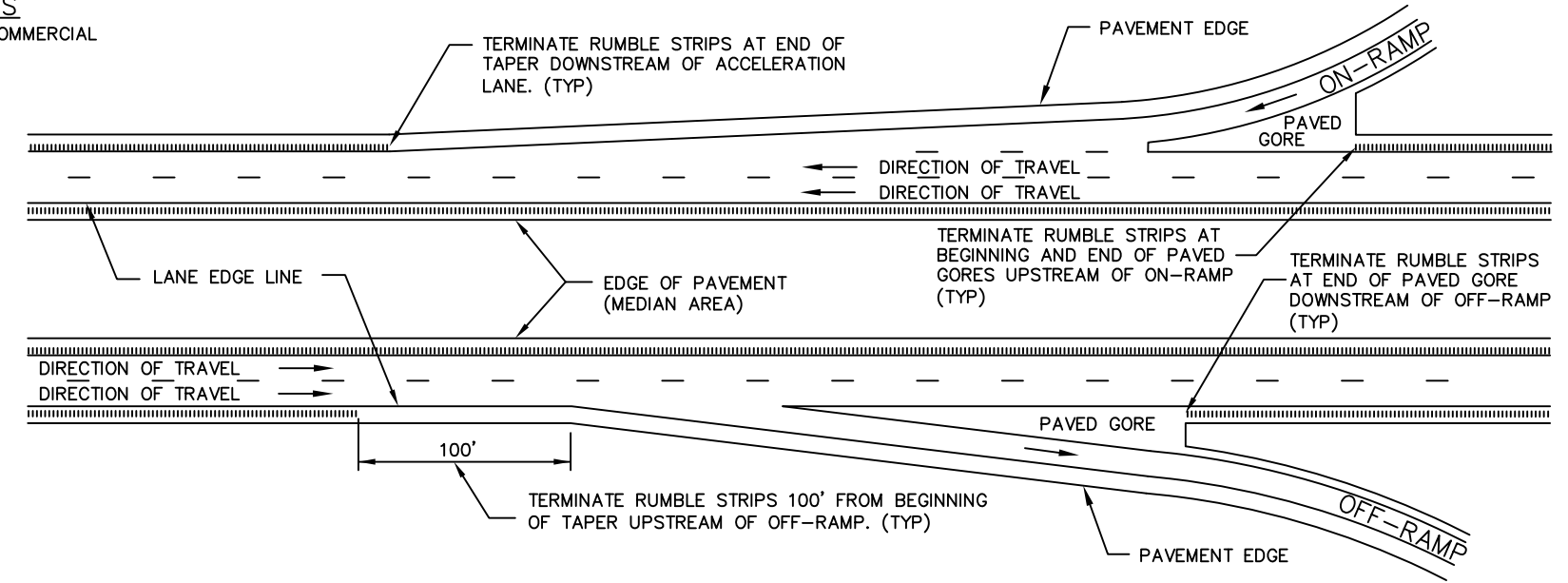


**RUMBLE STRIP LAYOUT AT INTERSECTIONS**

APPLIES TO ALL SIDE ROAD INTERSECTIONS, PUBLIC TURNOUTS, COMMERCIAL ROAD APPROACHES, AND GANG MAILBOX TURNOUTS (WHERE BICYCLES ARE ALLOWED)

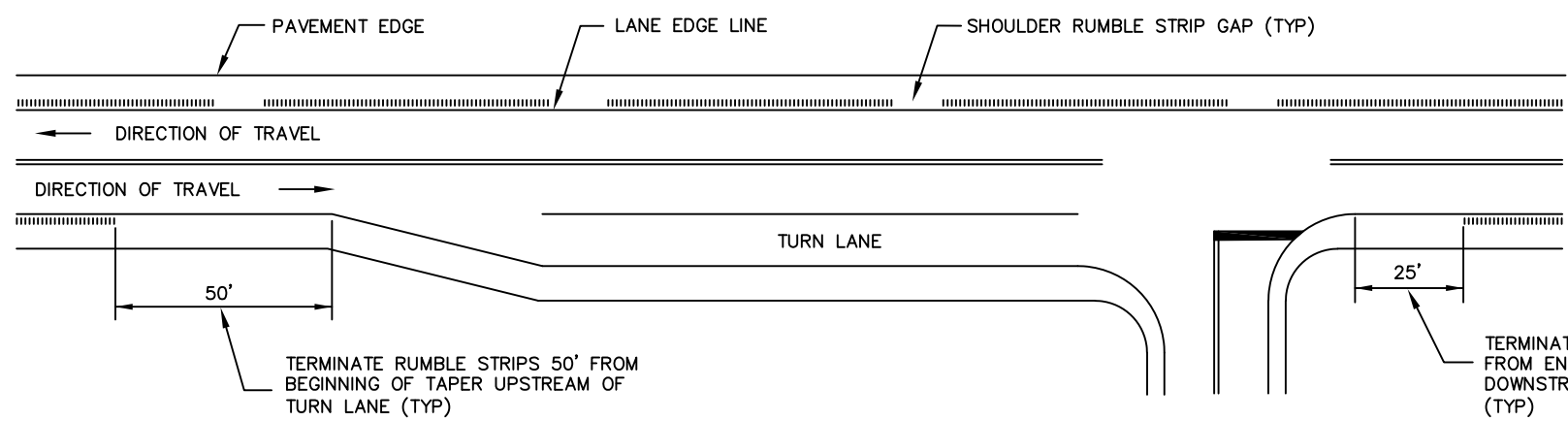


**RUMBLE STRIP LAYOUT AT RESIDENTIAL DRIVEWAYS**



**RUMBLE STRIP LAYOUT AT FREEWAY ON- AND OFF-RAMPS**

THIS DRAWING APPLIES TO BOTH PARALLEL AND TAPERED LANES (WHERE BICYCLES ARE ALLOWED)



**RUMBLE STRIP LAYOUT AT RIGHT TURN LANES**

(WHERE BICYCLES ALLOWED)

Note: Drawing not to scale

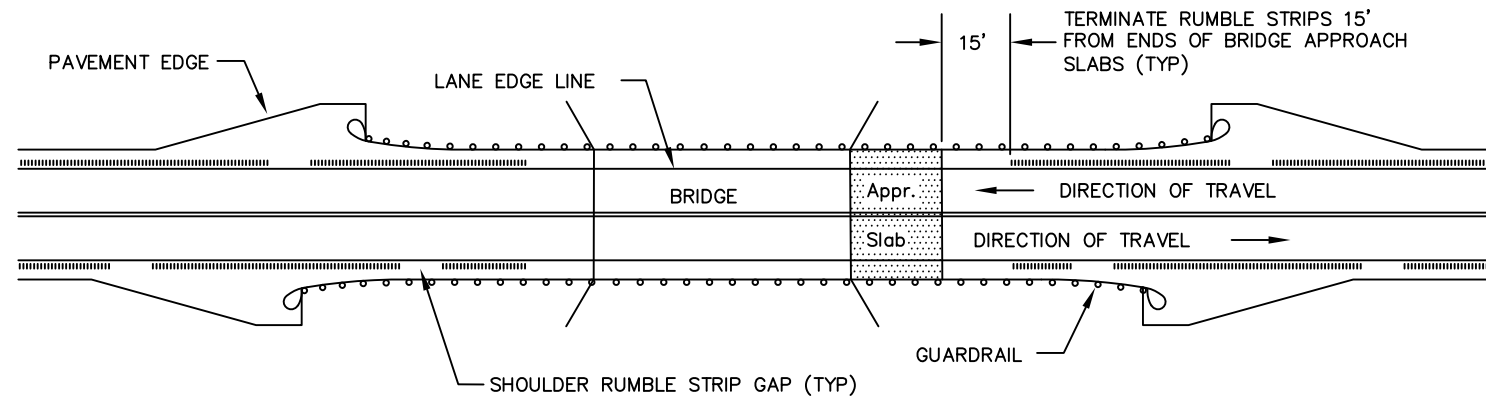
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ALASKA STANDARD PLAN  
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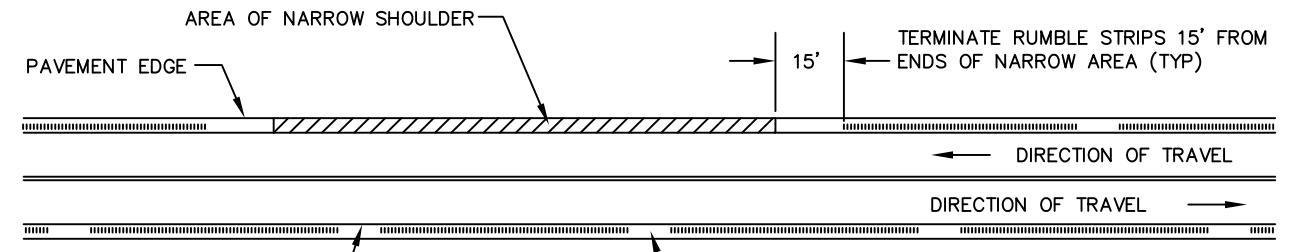
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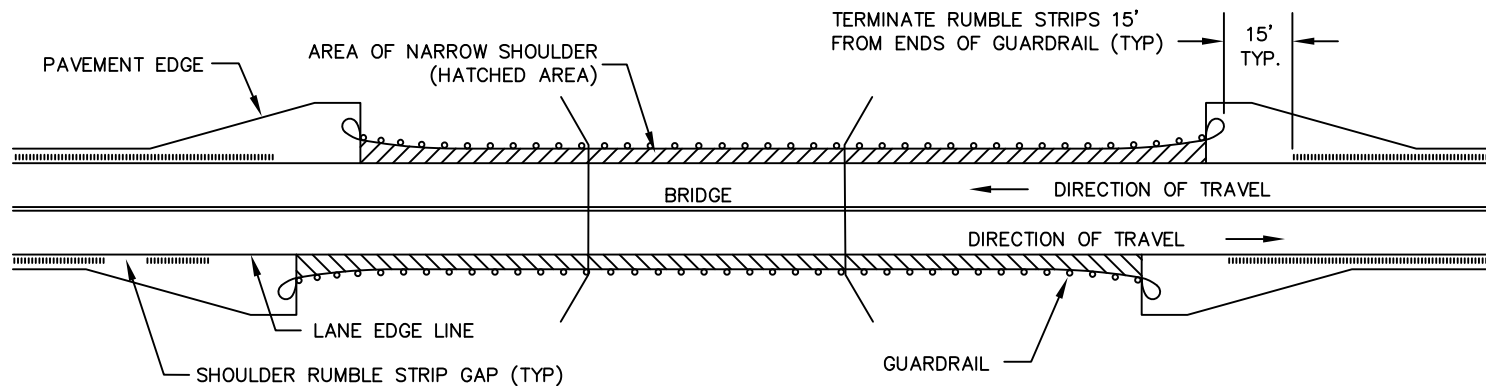
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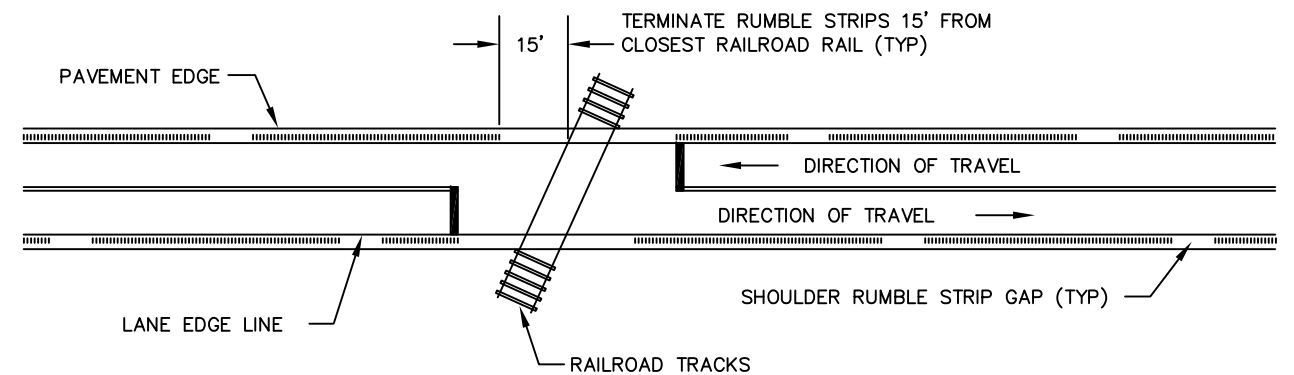
**RUMBLE STRIP LAYOUT AT BRIDGES WITH ADEQUATE SHOULDER**  
(WHERE BICYCLES ARE ALLOWED)



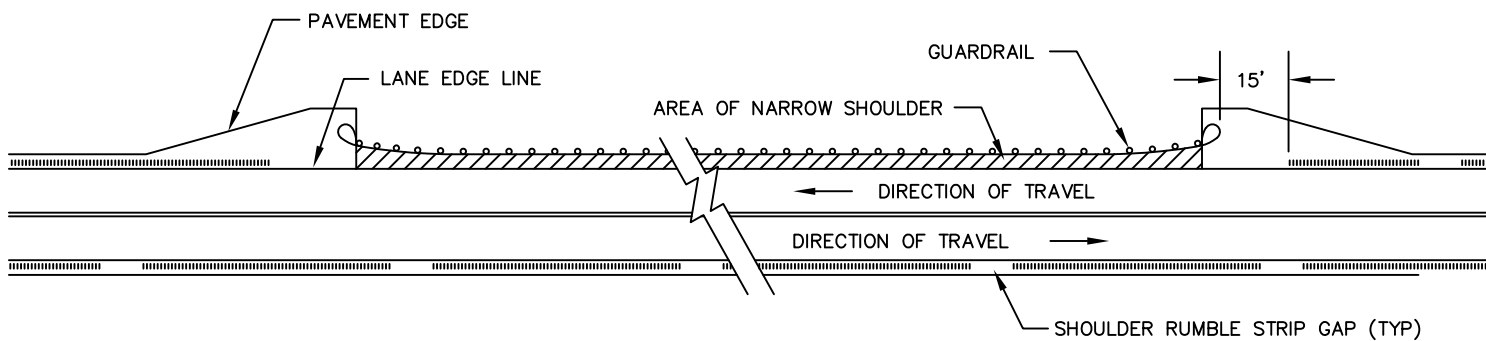
**RUMBLE STRIP LAYOUT IN AREAS WITH NARROW SHOULDER**  
(WHERE BICYCLES ARE ALLOWED)  
(SEE NARROW SHOULDER WIDTH NOTE THIS SHEET FOR DEFINITIONS AND TOLERANCES)



**RUMBLE STRIP LAYOUT AT BRIDGES WITH NARROW SHOULDER**  
(WHERE BICYCLES ARE ALLOWED)  
(SEE NARROW SHOULDER WIDTH NOTES THIS SHEET)



**RUMBLE STRIP LAYOUT AT RAILROAD CROSSINGS**  
(WHERE BICYCLES ARE ALLOWED)



**RUMBLE STRIP LAYOUT IN AREAS WITH GUARDRAIL AND NARROW SHOULDER**  
(WHERE BICYCLES ARE ALLOWED)  
(SEE NARROW SHOULDER WIDTH NOTES THIS SHEET)

**NARROW SHOULDER WIDTH NOTES:**

A SIX INCH TOLERANCE IS ALLOWED (FOR DISTANCES OF 100 FT. OR LESS) FOR THE FOLLOWING MINIMUM REQUIRED CLEAR WIDTHS:

- a. AT LEAST 4' WHERE NO GUARDRAIL IS PRESENT.
- b. AT LEAST 5' (TO FACE OF GUARDRAIL) WHERE GUARDRAIL IS PRESENT.
- c. NO MINIMUM WHERE BICYCLES ARE PROHIBITED.

Note: Drawing not to scale

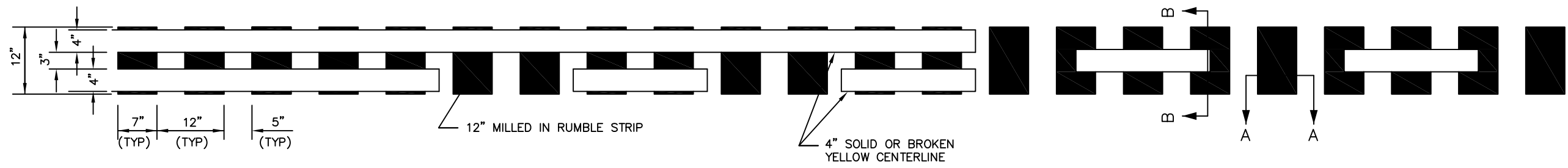
**State of Alaska DOT&PF**  
**ALASKA STANDARD PLAN**  
**MILLED RUMBLE STRIPS**  
**SHOULDER DETAILS**

Adopted as an Alaska Standard Plan by: *Carolyn Morehouse*  
Carolyn Morehouse, P.E.  
Chief Engineer

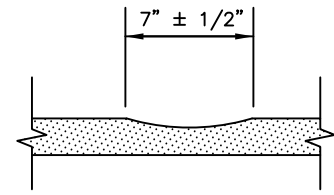
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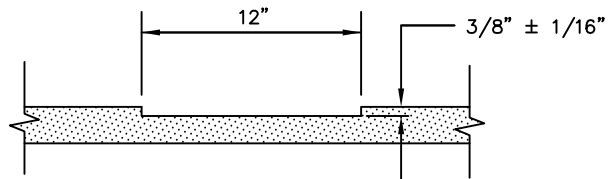
Next Code and Standards Review date: 07/17/2030



CENTERLINE RUMBLE STRIP PLAN VIEW



SECTION A-A



SECTION B-B

CENTERLINE RUMBLE STRIP NOTES:

1. PERFORM ALL STAKING AS NECESSARY TO INSTALL RUMBLE STRIPS IN ACCORDANCE WITH THE PLANS, THESE DETAILS, AND THE FOLLOWING NOTES.
2. DO NOT INSTALL RUMBLE STRIPS IN THE FOLLOWING INSTANCES:
  - A. BRIDGE DECKS
  - B. BRIDGE APPROACH SLABS
  - C. PAVEMENT LESS THAN 2 INCHES THICK
  - D. PAVEMENT THAT HAS ALLIGATORING, FATIGUE, CRACKING, OR IN POOR CONDITION
  - E. PAVEMENT JOINTS
  - F. INTO LANE EDGE LINE STRIPING
3. WHERE INSTALLED, CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS REGARDLESS OF CENTERLINE STRIPING CONFIGURATION. BOTH PASSING AND NO-PASSING PORTIONS OF ROADWAY WITHIN THE LIMITS OF THE CENTERLINE RUMBLE STRIP INSTALLATION SHALL BE MILLED.
4. CENTERLINE RUMBLES MAY BE EXTENDED INTO PAINTED MEDIANS WHERE A DOUBLE YELLOW STRIPE SEPARATES OPPOSING TRAFFIC. WHERE CENTERLINES SPLIT TO CREATE A LEFT TURN LANE ALONG A RURAL HIGHWAY, THE RUMBLES SHOULD BE PLACED ALONG BOTH PORTIONS OF THE CENTERLINE.
5. DO NOT INSTALL CENTERLINE RUMBLE STRIPS IN A TWO-WAY LEFT TURN LANE.
6. DO NOT INSTALL CENTERLINE RUMBLES WHEN THE COMBINED LANE AND SHOULDER WIDTH IN EACH DIRECTION IS LESS THAN 14'.
7. BREAK CENTERLINE RUMBLES FOR ALL SIDE STREET AND COMMERCIAL ROAD INTERSECTIONS WHERE THERE ARE LEFT TURN LANES.
8. CENTERLINE STRIPING SHALL BE RE-ESTABLISHED FOLLOWING MILLING OPERATIONS IN ACCORDANCE WITH SECTION 670, "TRAFFIC MARKINGS". 60 MIL SURFACE APPLIED METHYL METHACRYLATE PAVEMENT MARKINGS SHALL BE INSTALLED ON ALL AREAS FOLLOWING CENTERLINE RUMBLE STRIP INSTALLATION WHERE CENTERLINE RUMBLE STRIPS ARE APPLIED.

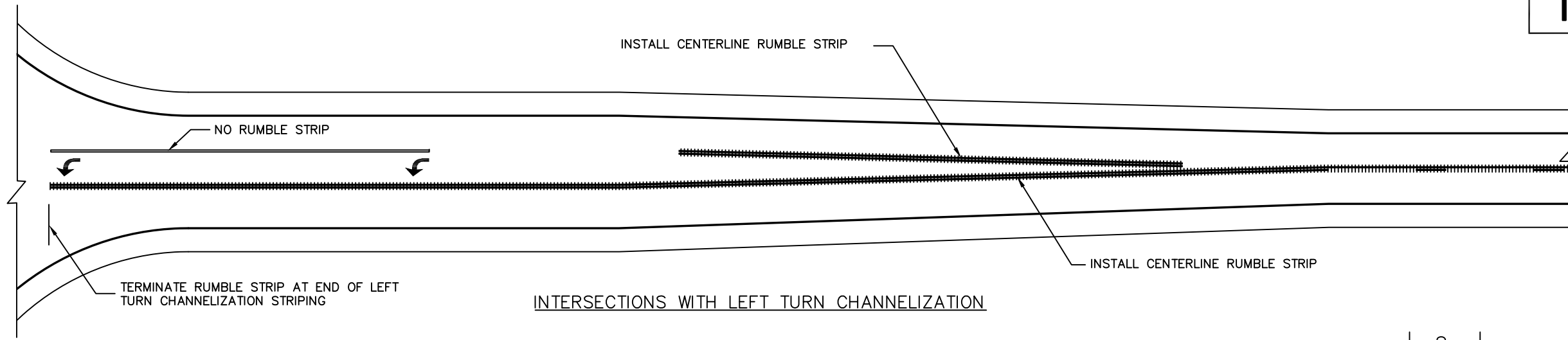
Note: Drawing not to scale

State of Alaska DOT&PF  
ALASKA STANDARD PLAN  
MILLED RUMBLE STRIPS  
CENTERLINE DETAILS

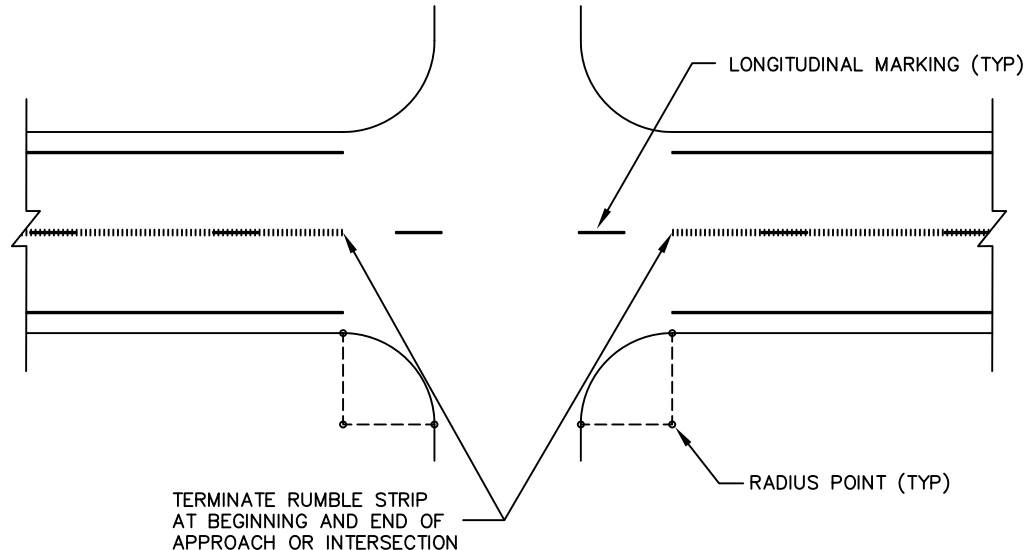
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Chief Engineer

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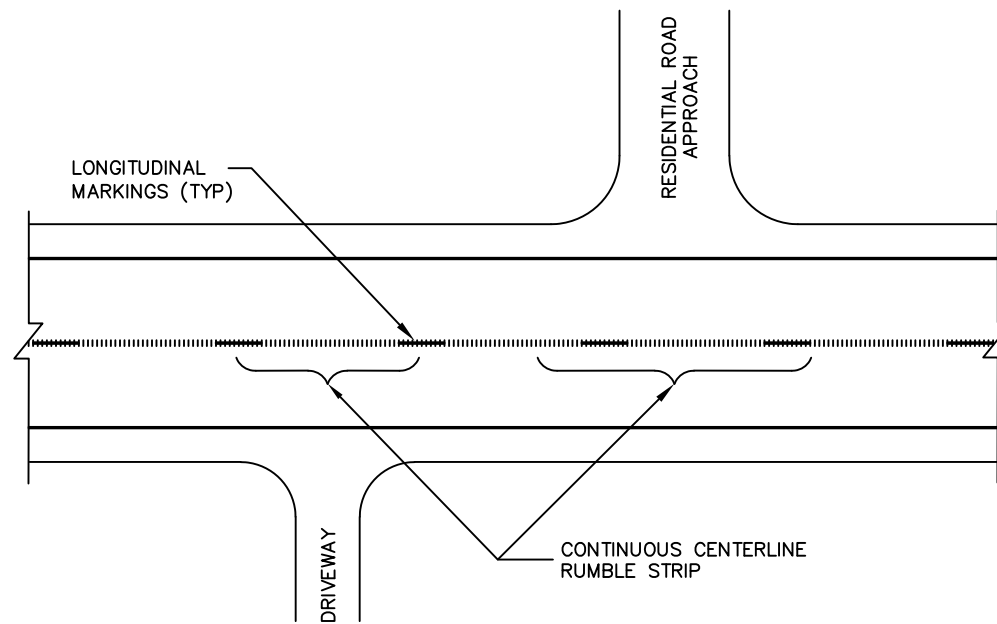
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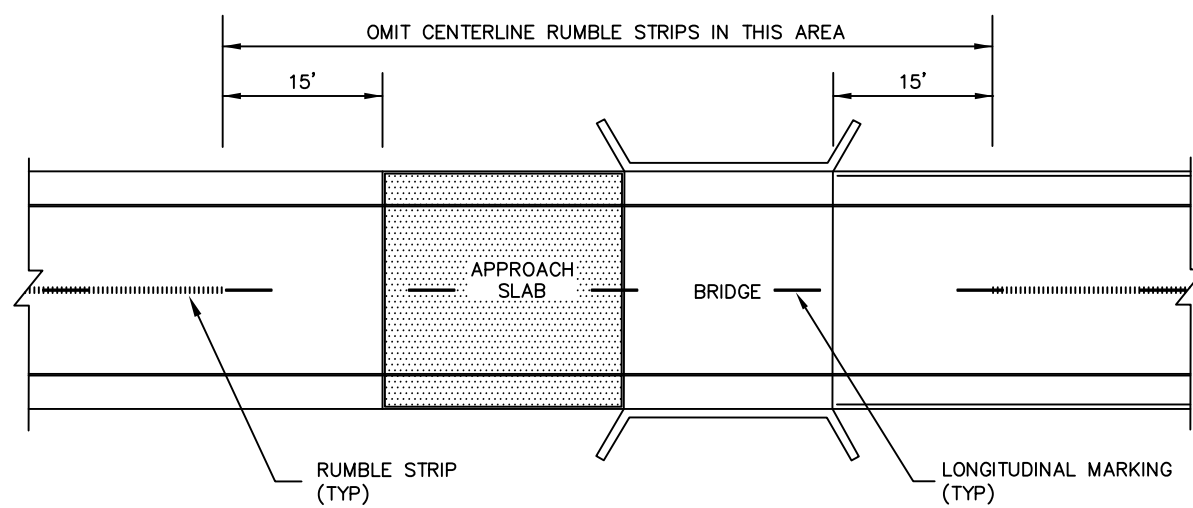
INTERSECTIONS WITH LEFT TURN CHANNELIZATION



HIGHER VOLUME INTERSECTIONS AND COMMERCIAL APPROACHES



NON-COMMERCIAL ROAD AND DRIVEWAY APPROACHES  
(DO NOT BREAK FOR THESE ACCESS POINTS)



BRIDGE

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ALASKA STANDARD PLAN  
MILLED RUMBLE STRIPS  
CENTERLINE DETAILS

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